



Perth-Andover Downtown Plan

Final Report

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Executive Summary

The St. John River Valley is an idyllic setting for a small community. Nestled in the heart of the Appalachian Mountains near the mouth of the Tobique River and wilderness areas, ripe for great fishing and hunting the area is truly beautiful.

The community of approximately 1,700 residents serves a regional population of 6,000. Established by English soldiers in the 1700's and amalgamated in 1966, the community adorns both the banks of the St. John River. It is a community with strong attachments to its environment and the river that runs through it. Volunteers organize a number of yearly events including; Laree Creek Hullabaloo, Gathering of the Scots, NB Pro Bass Tournament, Winter Carnival, Rocktober Fest and Tobique First Nation (Negootkook) Powwow. The merchants and citizens want to retain their traditional way of life and their traditional downtown.

The Village of Perth-Andover is at a crossroads. Without a clear direction forward the fate of the traditional downtown is in serious jeopardy. Many of the province's downtowns continue to struggle to stay vital in the face of changing retail operations, particularly for rural communities trying to stem outward migration. However, Perth-Andover is also challenged recently with disastrous flooding events as a result of ice jams on the St. John River. While a plan was recently implemented to relocate affected residential buildings to higher ground the business and institutional owners nervously wait for the possibility of yet another devastating flood. They do not know whether to invest, flood proof or relocate their buildings. The loss of more businesses could signal the end of what once was a thriving downtown. This urban design plan presents a unified vision for the future of the Village of Perth-Andover.

The planning process offered three options for dealing with the unpredictable flooding:

1. Install a river wall barrier
2. Raise the downtown elevation with fill
3. Relocate the downtown to the Tribe Road (Highway site)

After reviewing the pros and cons of each option and a review of best practices in other flood prone communities, the BIA and Council selected the option to raise the elevation of Main Street with fill material. This scheme involves relocating the Main Street back from the river and creating a ground plane or "building pad" above the provincially set high water mark. Existing buildings in good state would be relocated to the new building pad, derelict buildings would be torn down and new buildings would be constructed. In many ways the scheme is a "do over" and a wonderful opportunity to instill the best of small urban downtown principles. By clustering buildings in a continuous tight arrangement, all facing the river, densification can be achieved. Maintaining a tight, easily walkable downtown is paramount for pedestrians and retail sales. Clustering of services like the post office, library, banks and hardware stores leads to spontaneous purchases from walk by traffic. A dense, recognizable core is also ideally suited for tourism, especially if building facades are reflective of traditional architecture styles and materials.

A mixed-use of residential and retail-institutional use is recommended in creating a vibrant and sustainable downtown. Ample on street parking and rear lot parking provides a service level that matches the expectations of rural customers. A welcoming pedestrian plaza at the bridge will become a



Coronation Park Master Plan - Proposal

focal point and signature space for the downtown with connections to the trails in behind. A pleasant waterfront “green” occupies the riverfront with meandering paths and generous landscaping.

The Rural Plan of 1997 needs to be updated to encourage commercial development in the downtown and for the 10 businesses located outside the BIA, in Andover, which should be encouraged to participate in densification of the commercial core for the benefit of all.



Business Case

Preservation of the downtown is akin to retaining the very identity of the community. As the heart and soul of the Village, the downtown is also valued for the existing infrastructure and facilities that account for approximately \$2,000,000 in annual property taxes.

The vision for downtown Perth-Andover presents a fix to the threat of the future flooding; based on provincial safety factors for flooding levels. The vision retains the best of the assets and removes those that are detrimental to new investment. The plan outlines a bold move up and away from the riverbanks to higher ground while maintaining the riverfront views. The plan is inclusive in that it supports the adjacent businesses on the Andover side left isolated by the repositioned homes. Implementation of the plan will not only take funding, but also strategic planning in the assembly of new property via layouts, lease and ownership arrangements, and assessments on relocation and flood proofing feasibility. The plan can be implemented in stages as funds and investments become available, starting at Station Street and working towards the river.

This is a long term, sustainable vision; however, action must be taken immediately to clarify for property owners the risks including the status quo of further loss of critical mass and feasibility of flood proofing. The business community has spoken and they want to rebuild and reinvest in a new revitalized downtown in its present location.



1.0 Introduction

The Village of Perth-Andover is at a crossroads. Without a clear direction forward the fate of the traditional downtown is in serious jeopardy. Perth-Andover is challenged with disastrous flooding events as a result of ice jams on the St. John River. While a plan was recently implemented to relocate affected residential buildings to higher ground the business and institutional owners nervously wait for the possibility of yet another devastating flood. They do not know whether to invest, flood proof or relocate their buildings. Following the most recent flood in 2012, a number of businesses chose not to reopen, others relocated to the Highway Commercial zone surrounding Tribe Rd. where there is no risk of flooding. The loss of more businesses could signal the end of what once was a thriving downtown. The community of approximately 1,700 residents serves a regional population of 6,000. Established by English soldiers in the 1700's and amalgamated in 1966, the community adorns both the banks of the St. John River. It is a community with strong attachments to its environment and the river that runs through it.

The downtown serves as the heart of the community; representative of its history.

While a decisive program was in place for residents affected by the flooding, financial assistance for businesses was less clear. The Village of Perth-Andover retained the Glenn Group to explore solutions for the business improvement area (BIA) and develop a vision of the preferred option.

This urban design plan presents a unified vision for the future of the Village of Perth-Andover.



Downtown Perth-Andover during the 2012 flood



1.1 Background

The Village of Perth- Andover has endured numerous floods over the past 40 years. Recently the flood levels have risen. Businesses have no clear solution for managing flood risk. Many local businesses have closed for good while others have relocated to higher ground outside the downtown. The out-flux of businesses has left a void in the downtown with little support for the remaining businesses amongst a number of abandoned properties.

The community of Perth-Andover has suffered a loss of morale and vibrancy due to the current state of its downtown. It needs a clear vision to restore the “heart of the community” that its downtown once provided. Remaining businesses are looking for compensation for the damage to their properties. A plan must be devised that illustrates a cohesive vision for the future of the downtown which supports existing businesses, instills confidence in potential new investors and re-invigorates the community perception of the downtown.

1.2 Purpose

The purpose of this plan is to create a vision for the Business Improvement Area of Perth-Andover which supports enterprise, draws new investment and reinvigorates the community perception of its downtown.

1.3 Goals and Objectives

- To create commercial density to support enterprise
- To reinstate an economic and social heart in the Village of Perth-Andover
- To develop a plan for reinvestment in the businesses of Perth-Andover
- To develop viable options for businesses which fall outside of the BIA
- To provide public space for gatherings, civic events and fostering social activity in the community
- To revitalize the aesthetics of the downtown through attractive, historically appropriate building materials, architectural detail and streetscape enhancements
- To develop a plan that promotes one-stop shopping for all retail and service needs in the community
- To attract new commercial investment in Perth-Andover
- To develop a plan for Perth-Andover’s BIA which will reasonably withstand future flooding of the St. John River.

2.0 Options

2.1 Existing Conditions

- 6 busiensses did not reopen after 2012 flood
- Aproximately 15 businesses in operation in BIA
- BIA boundaries, established in 1989:



Perth-Andover Downtown Plan

From Village Automotive at the North End of Perth Main St. to the Northwest Corner of the Irving. From the river to the eastern right of way of Station Rd. and Second Street. And north along the Westerly limits of the CP right of way back to the intersection of Route 105

(for a detailed description see Appendix II; "By-Law #54, schedule A, schedule B)



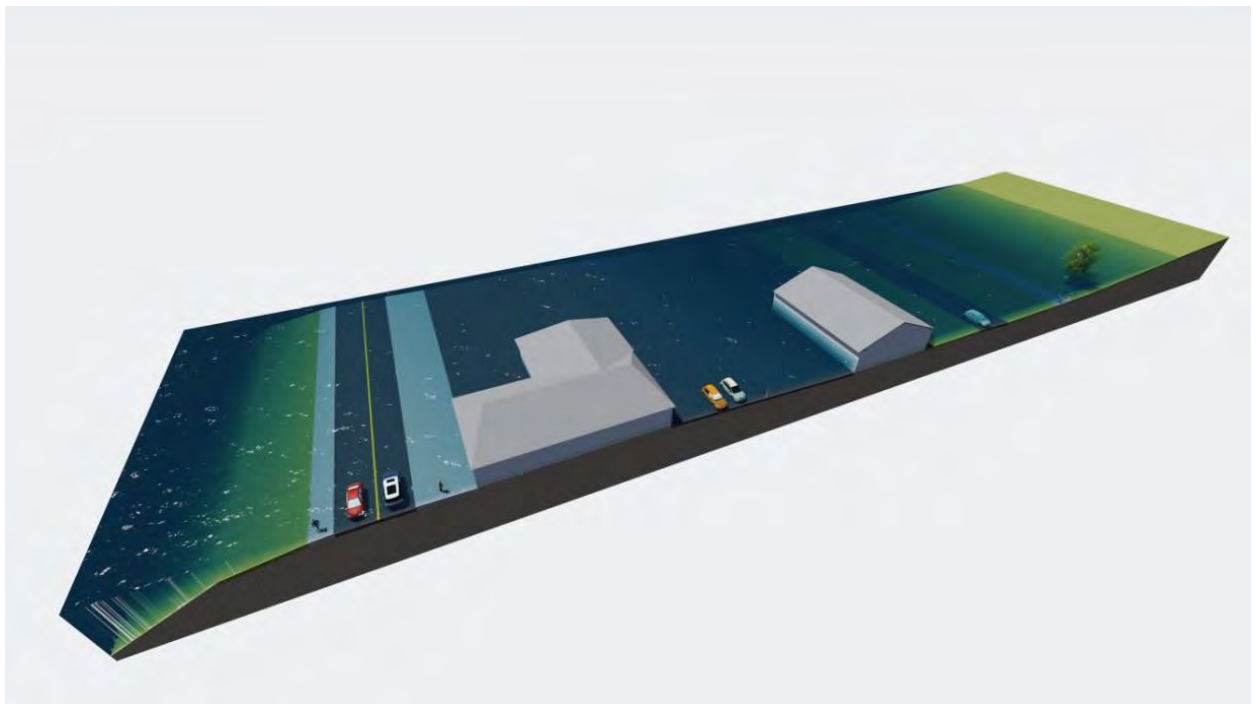
Business Improvement Area (BIA) Boundary

- Current elevation of downtown averages 79.5 m
- 2012 flood level: 80.24 m
- New flood level set by province at 81.25 m
- 12 Businesses fall outside of BIA, but still in flood zone, 10 of which are on the Andover side
- A few businesses have already relocated to higher ground
- BIA is home to the Library and Post Office
- BIA currently subject to seasonal flooding
- Commercial development has been actively growing along Tribe Road for more than 15 years
- Residential units of various types exist in BIA
- A boardwalk was constructed along the river bank with benches, street lights and trees. It is popular with residents and is aesthetically appealing.





Typical cross section of existing downtown core from St. John River to NB trail



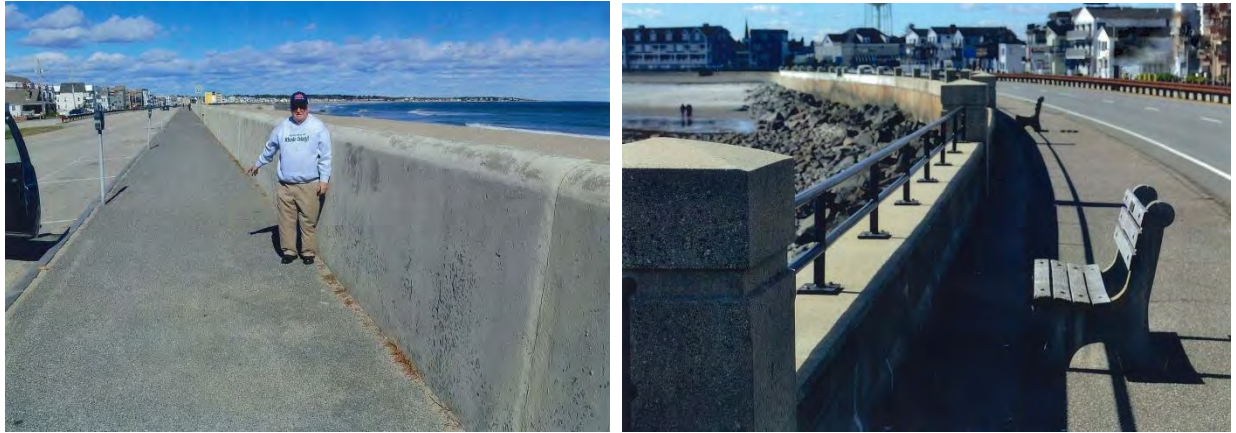
Typical Cross Section of existing downtown core during 2012 flood high water level

2.2 Option 1: Install River Wall

A river wall was suggested by residents of Perth-Andover who had seen successful examples in seaside towns including Fort Fairfield, ME. With this option the existing town would be protected in its current state. Detailed studies would need to be done to determine the extents of the wall to ensure flood waters did not breach at any point. The height and width of the wall would also be subject to further study.



Perth-Andover Downtown Plan



Examples of Floodwall in Fort Fairfield, ME

Although this option would mean the downtown could exist where it is, it would have a significant impact on the landscape, blocking views of the river which is the primary feature around which the village is built. To help moderate the impact, the boardwalk would be elevated to improve pedestrians' view of the river.



Illustration of downtown with river wall and raised boardwalk



Pros	Cons
Don't have to relocate buildings	May limit views to river from the street
Increased park space along water	Extend solid wall along bridge to flood level height
Don't have to relocate Perth Main St.	Still have to pump out water that gets behind the wall
Can be used as an iconic design feature	

2.3 Option 2: Raise the Downtown with Fill Material

This option involves bringing in large amounts of fill to raise the finished ground elevation. By elevating the downtown, the buildings would be protected from future flooding. Although the most expensive option, this would preserve the historic and social connections to the downtown that locals know and love while maintaining the views of the St. John River.

Elevating the downtown can be done to varying degrees. The greater the area of fill, the more developable land will be available. This is an expensive endeavor as buildings will need to be moved, new foundations poured, and new services installed.

Below are three options for elevating the downtown with fill. They vary in the degree to which the area is filled and have correspondingly increasing associated costs.

- 1) Elevating Main Street and Fronting businesses only
- 2) Elevating to Station Rd.
- 3) Elevating from the river bank to the NB trail.

2.3.1 2A: Raise Buildings on Main Street Only

By raising the elevation of Main Street and the businesses fronting Main Street, the majority of businesses and services would be protected from future flooding.

In order to gradually increase the elevation sloping back from the river bank, it is recommended that Main Street be relocated 30 m back from its current position. As such, Main Street would also be above the flood level so day to day commerce could continue during flood periods. Many buildings would have to be relocated due to the fact that Main Street would also be relocated.

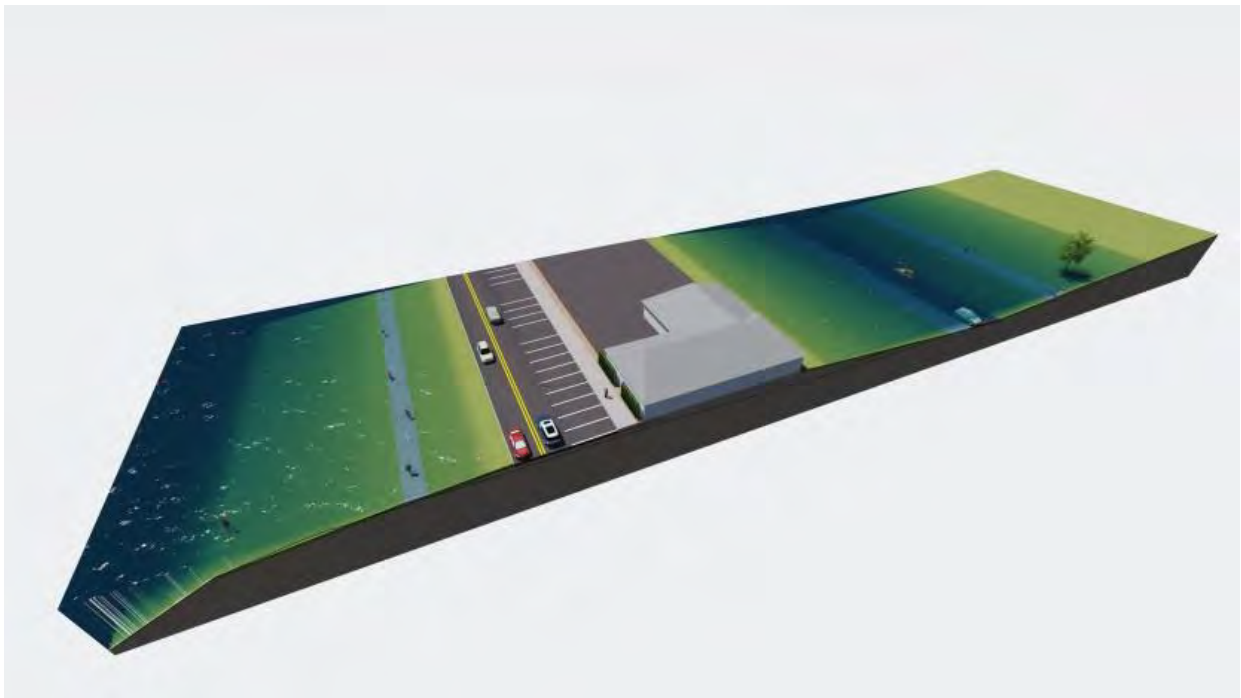
Vacant buildings along Station Rd. would be demolished to allow for improved densification in the new downtown plan. A gradual slope would be achieved to connect the elevated land back to the existing grade along Station Rd.



Perth-Andover Downtown Plan



Cross Section of typical downtown with fill material elevating only Main Street Frontage



Cross Section of typical downtown with businesses fronting Main Street elevated, during 2012 flood levels



Pros	Cons
Relatively cost efficient	Limited developable land
Improved green space along waterfront	Cost to relocate Main Street
More attractive Main Street	Relocate buildings
	Station Road will still flood
	Need to relocate buildings on Station Road (still in flood zone)

2.3.2 2B: Elevate to Walking Trail Level

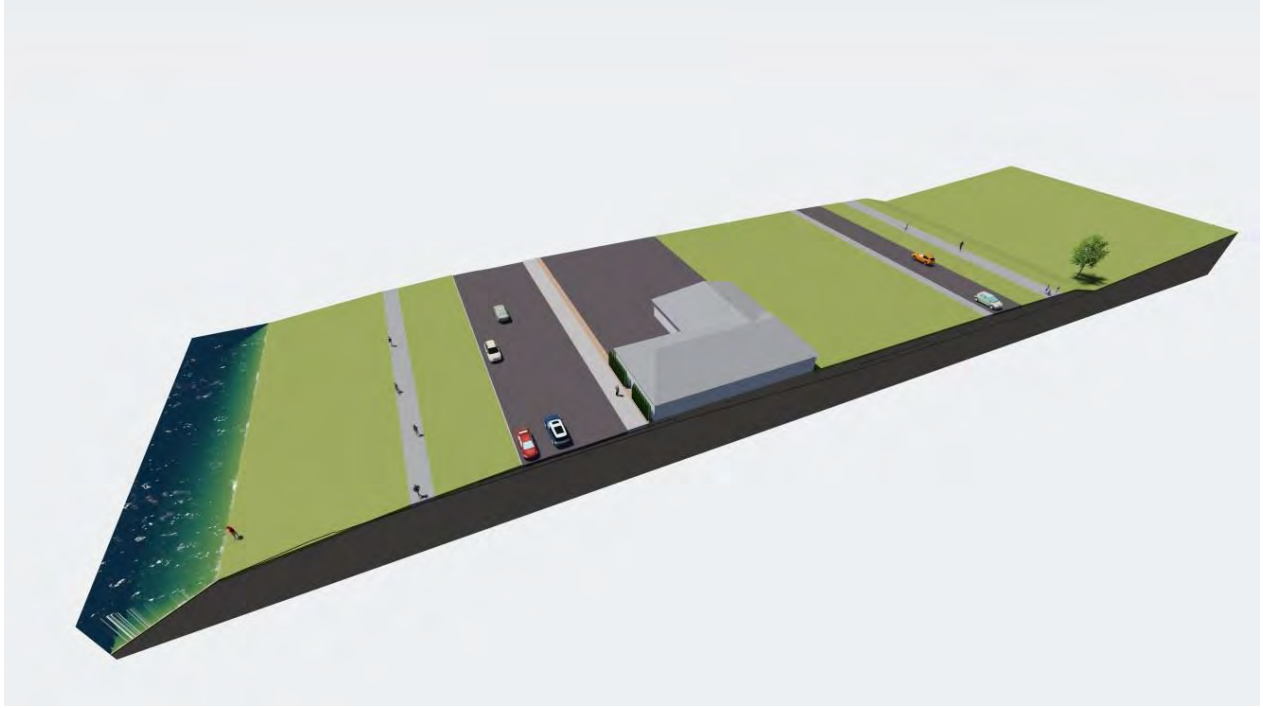
This option offers the most developable land for the downtown with the potential for a complete redesign of downtown Perth-Andover, retaining its charm and location while improving the function. This concept requires the greatest amount of fill, elevating the downtown with a gradual slope back from the stabilized riverbank all the way to the NB trail. As such, this is the most costly option.

In this option Main Street would be set back 30 m from its current location with the newly relocated and constructed buildings fronting the street. This opportunity allows for a complete redesign of the downtown to achieve improved densification. Outdoor open spaces can be incorporated for hosting civic events, passive recreation, outdoor dining.

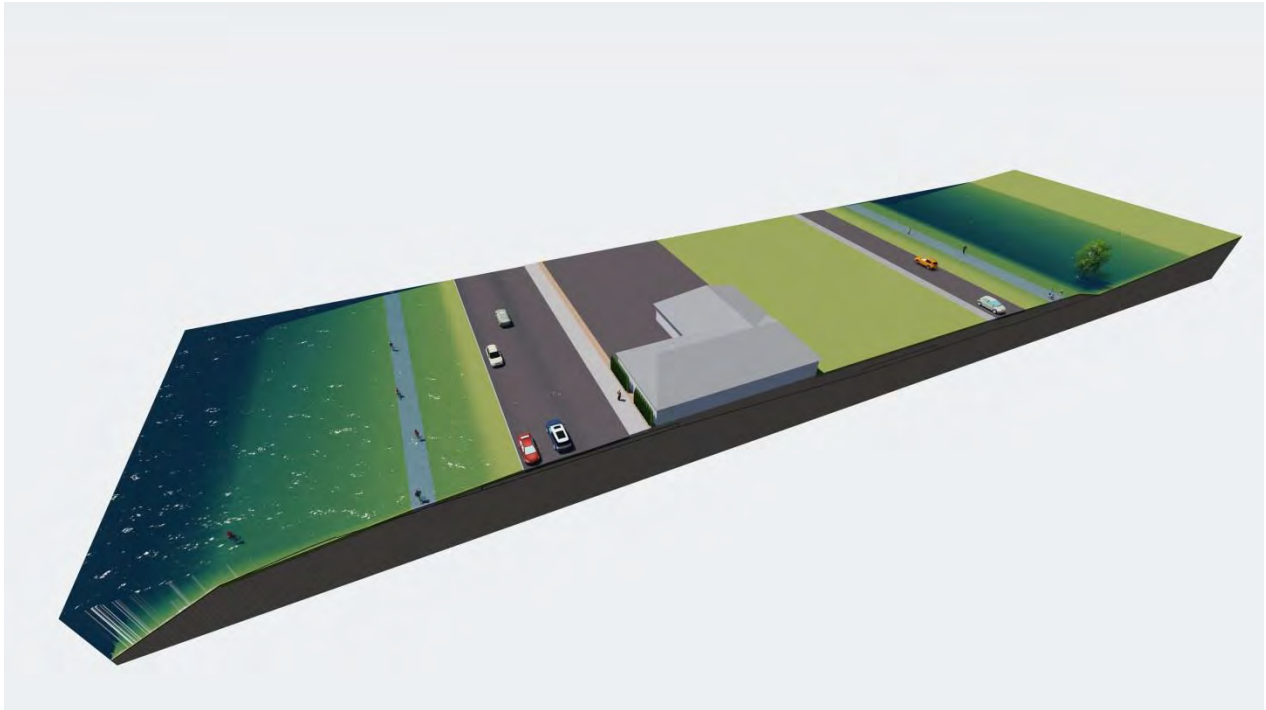
In this scheme, Station Rd. would be elevated as well so that the entire downtown would be above flood level limiting the interruption to commercial activity during such floods. Property fronting Station Rd. could be developed for commercial real estate or for additional parking allowing for deeper building lots along Main Street.



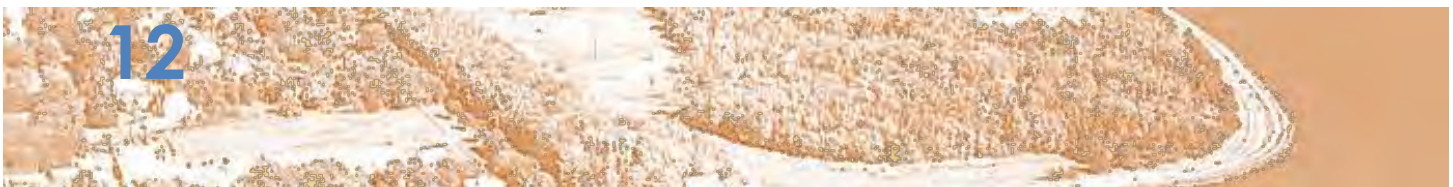
Perth-Andover Downtown Plan



Cross Section of typical downtown elevated with fill from river to NB trail



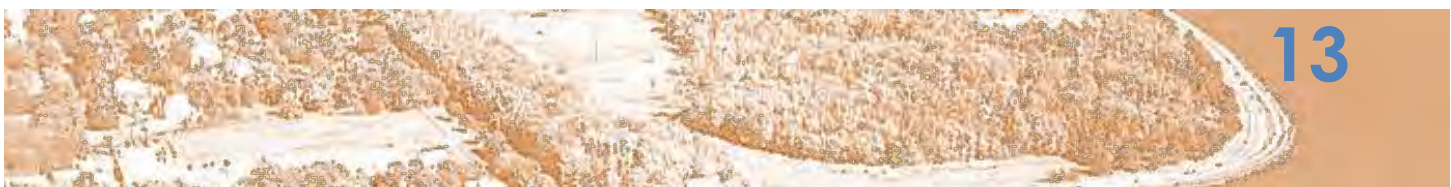
Cross Section of typical downtown elevated with fill from river to NB trail during 2012 flood levels



Pros	Cons
Improved green space along waterfront	Cost to relocate Main Street
More attractive Main St.	Relocate buildings
Unobstructed view of river	Greatest amount of fill- expensive
Greatest developable land opportunity	
Can develop Station Rd. as well	
Maintain atmosphere of livable downtown	

2.3.3 2C: Install Retaining Wall along Station Rd.

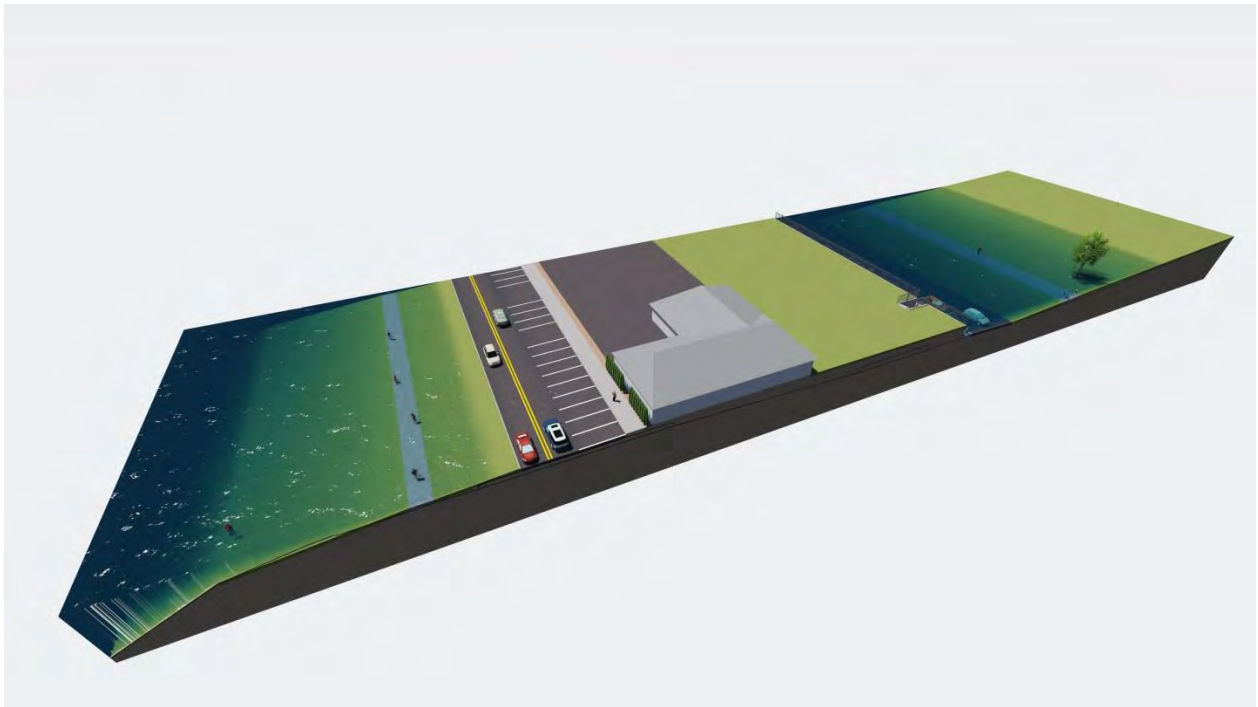
This option is similar to option 2B, maximizing developable commercial land from Main Street to Station Rd. The difference here is that a retaining wall would be built along Station Rd. to transition back to the existing elevation. The benefit of this option is in the savings of the costly reconstruction of Station Road and any services located under it. However, in this concept Station Road itself would still be subject to seasonal flooding. The idea of a large retaining wall along Station Road which would cast shadows on the road and sidewalk and restrict views to the commercial space behind it may not be a popular choice for many residents. There are, however options that could be explored to take advantage of the wall, dressing it and turning it into a feature wall rather than a detractor.



Perth-Andover Downtown Plan



Typical Cross Section of downtown with fill from the river edge to Station Rd. at its current elevation



Typical Cross Section of downtown with fill from the river edge to Station Rd. at its current location. (Shown at 2012 flood level)

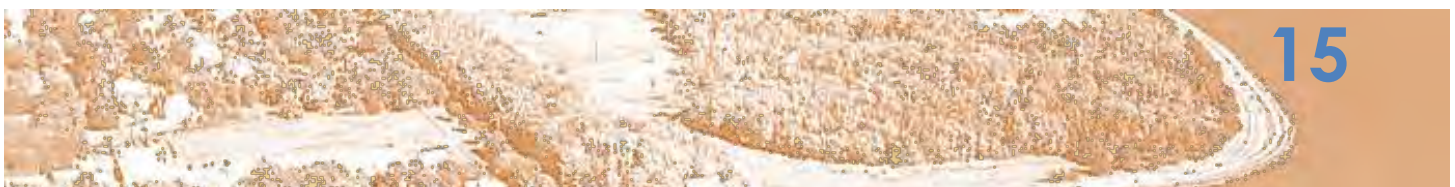
Pros	Cons
Improved green space along waterfront	Cost to relocate Main St.
More attractive Main St.	Buildings have to be relocated

2.4 Option 3: Relocate Downtown to New Highway Site

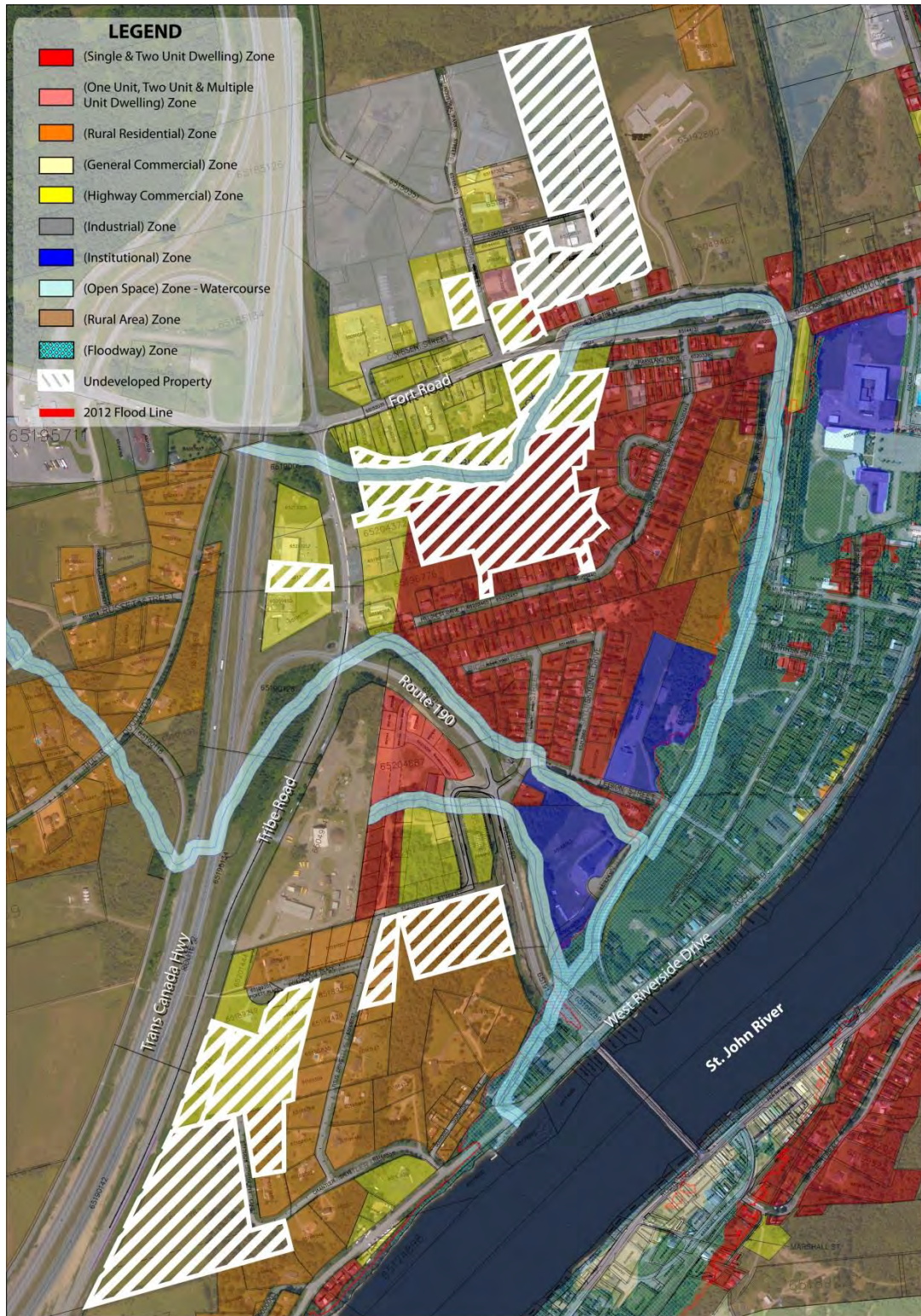
There has been a migration of businesses to the lands along Tribe Road, referred to as the Highway Commercial Zone, prior to the adoption of the last Rural Plan for Perth-Andover. After the flood of 2012 more businesses chose to make the move to escape the flood waters.

Although limited, developable commercial land remains. In this area the land is attractive to business owners as there is no risk of flooding and other businesses have already established the area. It is in close proximity to the highway which services rural clientele. However, the growth of this area has not been guided by any master planning and has been ad-hoc. It does not have the romantic connections to the river which make the current downtown so picturesque and it lacks the historic and social connection that generations of residents have to the downtown. Although this area may satisfy some commercial demands for the community, its growth potential is limited and it is not the “heart of the community” that the existing, or any traditional downtown is.

Below is an illustration of the lands surrounding Tribe Road with an emphasis on the Commercial and Industrial Zones. As illustrated (white hatching) there is limited land available for development, much of it is residential or interrupted by a watercourse, and there is no critical mass. The best available land is at the south end of Tribe Road, and disconnected from the existing development in the area. If this site was chosen for further expansion it would contribute further to the densification problem and lack of identity of a downtown.



Perth-Andover Downtown Plan



Zoning Map of Tribe Road area highlighting developable land parcels.



Pros	Cons
Long-term will not be affected by future flooding	Limited available land to expand
Some businesses have already relocated there	Traditional village atmosphere and river heritage will be further eroded
Increased exposure/ accessibility to highway traffic	Will cripple remaining businesses on Main Street
	What happens to the old downtown space?

2.5 Business Owners' Preference

A unanimous decision was reached to pursue Option **2B: Elevate the entire downtown with fill from the river to the walking trail**, with recommendations for future Highway Commercial development along Tribe Road.

3.0 The Preferred Option

3.1 Mission Statement

A well planned downtown that balances building density with public open spaces fosters a sense of community. The redesign of Perth-Andover will provide commercial opportunity while celebrating its history as the heart and soul of the village and region as a whole. An improved density in the downtown core will provide long term economic stability and growth, ensuring the downtown retains its charm for generations to come.

3.2 The Vision

The revitalized downtown will:

- Be filled to an elevation that will realistically withstand future flooding (0.5 m above provincially set flood line)
- Have a building density that supports a sustainable volume of businesses, and creates a pedestrian atmosphere
- Include outdoor pedestrian zones such as plazas, green space, patios
- Incorporate residential units both above retail space and with a townhouse development transitioning from the downtown to the residential properties
- Have ample on and off street parking for accessibility to businesses
- Will be oriented towards the village's best asset- the St. John River.



3.3 The Design



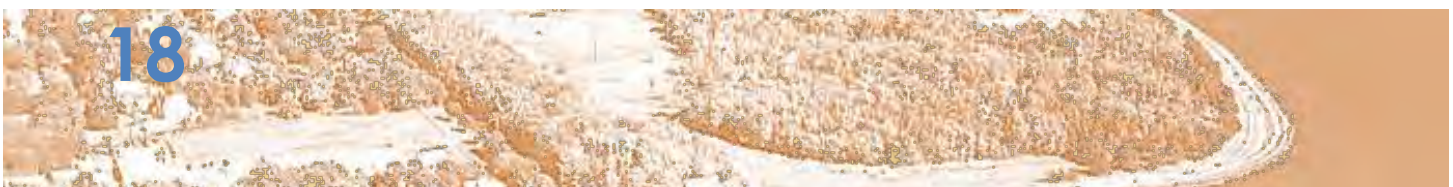
Plan View of concept for new Downtown Perth-Andover

3.3.1 Selected finished elevation

The design was developed based on a final elevation of 81.75 m for the downtown. The riverbank would gradually slope back to the new Perth Main Street located 30 m from the existing Main Street. This elevation would be achieved at the edge of the river side of Main Street and extend back past Station Road, tying into the existing elevation of the NB trail (which did not flood during the 2012 high waters). The newly elevated land would run from approximately the intersection of Station Rd. by the Irving to the beginning of Gulch Rd. to the north.

81.75 m was selected as the new downtown elevation to strike a balance between the proposed flood design elevation of 81.25 set by the provincial government and the desired elevation of residents at 82.8 m. This elevation is a full meter above the 2012 flood level and is approximately the elevation of the bridge as it intersects with either bank of the river. Should the river rise any higher than this, the bridge would be in jeopardy and it is therefore in the interest of all parties to ensure the river does not reach this level.

Although residents desire a level 2.5- 3 m above the 2012 flood level, this elevation is not practically achievable. Elevating the downtown by 2.5- 3 m or one entire building story is not only extremely expensive, but would also dramatically alter the landscape. The bridge would no longer be accessible and the relationship between the Perth and Andover sides of the river would be drastically affected. There may be encroachment on private lands on the hillside behind the NB trail and it would be difficult if not virtually impossible to tie into Gulch Rd, Hwy 105 and East Riverside Dr.



81.75 m was selected as a practical solution that meets the goals of the vision within realistic financial and engineering constraints.

3.3.2 Relocation of Main Street

The proposed concept relocates Perth Main Street 100 ft. (30 m) back from its current location. Due to the elevation change, existing services such as sewer and water lines could no longer be accessed or safely serviced. The road will need to be reconstructed to allow for fill and to gradually slope the land from the river to the desired elevation. By setting the road back 30 m, a gradual slope from the top of the existing riverbank to the design elevation can be achieved. It is practical to locate the new Main Street back 30 m so it will remain above flood levels and allow safe travel and continued commerce during flood periods.

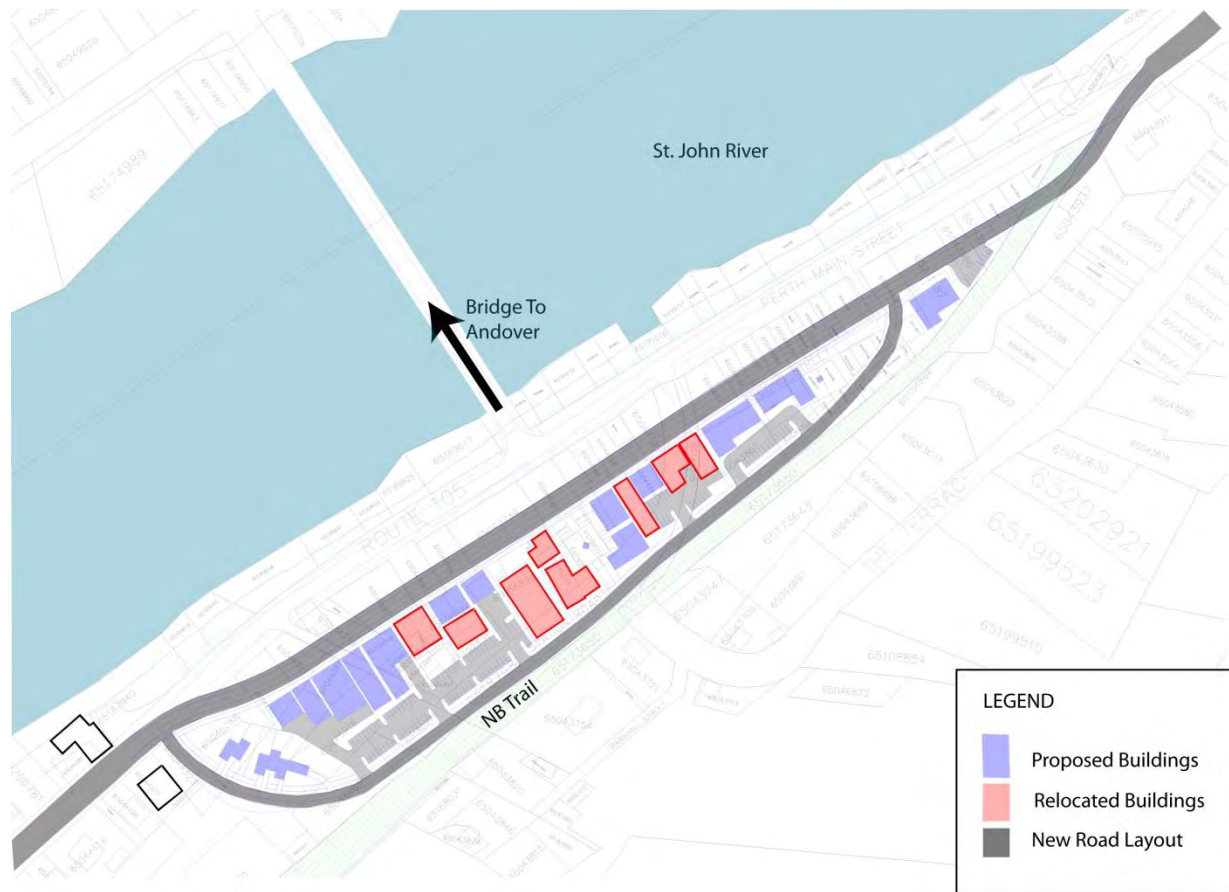
One of the most costly factors associated with the elevation change and reconstruction of the roads is the need to replace underground services. Such a change in elevation affects the services in a number of ways. With the relocation of the road, new infrastructure would be located under the new roads. With the change in elevation the existing pipes would be too deep to service the new streets and buildings.

Over time, as the old pipes deteriorate, they may collapse and cause the land above to collapse as well. For this reason, it is recommended that any abandoned pipes be filled with concrete to ensure the integrity of the soil above. This is less costly than digging up and removing them.

By relocating the Main Street, the riverfront land will be opened up allowing for a green space along the river. This ties in nicely with the concept developed for the Perth Residential Lands which included a riverfront park consisting of green lawns, shade trees and a walking trail.



Perth-Andover Downtown Plan



Overlay of new downtown location relative to existing

3.3.3 Parking

In order to foster a pedestrian environment, which benefits commerce, the new vision calls for an abundance of parking in close proximity to all businesses and services? Parallel parking has been proposed for both sides of Main Street. As well, parking lots have been located along Station Road with plenty of access points to Main Street and public plazas.

By providing ample parking, in close proximity to businesses, patrons will be inclined to use downtown as a one-stop-shopping opportunity. People will park and walk from destination to destination, increasing the likelihood of walk-by shopping which typically accounts for 75% of sales. They will also tend to lengthen their stay in the downtown, furthering the vitality of the downtown core. A pedestrian populated downtown is attractive to new visitors and snowballs, bringing more people into the downtown. It is easier to attract new investment in a downtown that is vital and active.



3.3.4 Relocation vs Construction of new building stock

Most of the existing building stock was severely damaged by the 2012 flood or has been vacant and untended. Five buildings have been identified as being of sound construction, worth preserving. They include:

- The library,
- The Bank of Montreal building, 686 East Riverside Dr.
- The Scotia Bank Building, 728 Perth Main St.
- Johnson's Pharmasave building, 694 Perth Main St.
- Lewis PharmaChoice, 704 Perth Main St.
- The Social Development NB building, Station Rd.
- Mixed use commercial/ residential building at 630 East Riverside Dr.
- Office building, 15 Station Rd.

These buildings would be relocated as part of the densification plan for the new downtown and set on new foundations. Once Station Rd. was reconstructed, the land behind the existing Main Street buildings would be elevated and then these buildings could be relocated.

The rest of the building stock would be replaced. The design includes 17 new buildings but this number would vary once more detailed design had been completed. This is an opportunity to create a harmonious, tasteful architectural theme amongst the buildings.

It is recommended that the buildings be a mix of 2 and 3 stories, allowing for offices and some downtown residential apartments. Residential space provides long term income potential which also helps offset the building costs. By including residential units in the downtown, there is a 24 hour presence which adds to vitality of the downtown core and creates an ideal sense of security. Downtown residents also create a constant demand for services and products.

In order to achieve a desirable densification, land must be reassembled. This would require cooperation between the Province, the Village of Perth-Andover and private land owners. This must be completed in the preliminary stage, prior to relocation and construction of any new buildings.

3.3.5 Proposed Office Building

A small office building has been proposed in the north end to accommodate professional practices. This building will serve as the north end anchor for the downtown and can provide space for multiple businesses. It should be no taller than 3 stories.

3.3.6 Residential component

In addition to above grade apartments, land has been allocated for approximately 6 townhouses at the south end of the downtown. This will provide options for people currently renting some of the detached and semi-detached homes in the downtown. A variety of housing options will cater to the varying demographic. Townhouses in this area provide a natural transition from the commercial core to the outlying residential development.



3.3.7 Public Open spaces

The quality of a community's open space speaks volumes about the values of its citizens and the success of its operation.

A downtown is further enhanced by public open spaces that cater to pedestrians. A variety of open spaces have been included in this plan from the riverfront green to urban plazas and patios. Public open spaces are ideal locations for holding civic events, sidewalk sales and markets. They encourage social interaction and create vibrancy in the downtown. Visibly populated downtowns attract more investment and visitation alike. Outdoor patio space makes the downtown attractive and is an appealing dining option for any restaurant patron.

A park that includes both a paved plaza with picnic tables and a passive green space has been located at the south end of town. This is an ideal location for the "snack shack" currently in the downtown. This park has been ideally located at the crosswalks to both the riverfront and NB trails making it an ideal connection between recreation and the downtown.

Upon arrival in the downtown one is greeted by an open, inviting civic plaza that is the service heart of the town. This is where the key institutional buildings would be located including the library, banks, post office and town hall. This plaza would be a pleasant blend of paved surface and soft landscaping. It would include open space as well as intimate pockets for seating. Besides civic events, this plaza would be visitors' first impression of Perth-Andover and should serve as the anchor from which everything else extends.

3.3.8 Building Materials, Architecture and Façade enhancements

The redesign of the downtown should be capitalized to revitalize the architectural theme of a traditional rural downtown. A number of the buildings being saved are constructed of a traditional red brick. This provides a good foundation for the architectural theme. Façade enhancements such as trim, casings, fascia, shutters, lighting, paint, awnings and shutters are relatively simple to apply to even existing buildings yet will have a significant impact. This opportunity will ensure consistent, high quality building forms. Buildings should have architectural features and patterns that provide visual interest at a pedestrian scale, and represent the character of the rural riverside community.





Sample Before and After of Tradition Downtown Retail buildings with Façade enhancements.

3.3.8.1 Colour and Materials

1. Traditional building materials should be applied to both new and relocated buildings to maintain the small town charm.
2. Materials and colours used in all buildings should be complimentary
3. Building colour schemes should include traditional, earthy tones
4. Building's exterior walls should be of high quality materials. Wood siding, wood shingles and brick should be the primary façade materials.
5. Secondary materials are encouraged to add visual interest, especially on the ground level

6. Walls not visible from Main St. may be of a secondary material to the front wall but must be harmonious with the dominant siding material.
7. Materials such as vinyl siding, metal siding and stucco should be avoided

3.3.8.2 Scale

1. Any building façade fronting Main Street that exceeds 12.2m in width should be broken into distinguishable ‘architectural parts’ using extending faces, changes in roof styles, changes in colour, and/or articulation in ‘bays’ to create an assemblage of architectural form.
2. Buildings should not exceed 3 stories in height.
3. Buildings exceeding 2 stories should consider stepped back upper stories in order to preserve the pedestrian environment.

3.3.8.3 Windows and Doors

1. Storefronts need large street level windows for displays and natural light. A minimum of 75% of ground floor street frontage should be windows and/or entry ways.
2. Wherever possible the front door should have barrier-free access
3. All windows and doors facing Main Street should have no less than 100 mm of casing trim surrounding them. The head casing for windows is typically larger than side casing or apron.
4. For traditional style, window muntins which create glass openings that have a larger height to width ratio should be considered.
5. Awnings and overhangs are encouraged to provide shade, weather protection and interest.
 - a. Awnings and canopies should be complimentary to building material and colour and highlight entrances.
 - b. Canopies should extend into the public realm a minimum of 1.5 m to sufficiently cover walkways.
6. CPTED principles should be employed in the design of entrances and exits (www.designcentreforcpted.org) of all buildings.
7. Exits should pay close attention to sightlines, lighting and maintenance issues.
8. Dark recesses should be avoided in all entries and exits.

3.3.8.4 Roofs

1. Eave and roof overhangs are encouraged particularly where they provide shelter over main entries
2. Cross gables, gable dormers and shed dormers are encouraged where possible on roofs to create visual interest.
3. Faux roofs (false roofs placed on facades to change the apparent proportions of the building) should look like real roofs whenever viewed from any angle on Main Street or from across the river.
4. Flat roofs may be appropriate, but variation in height and detail in the façade is encouraged.
5. All fasciae should be a minimum of 200 mm.
6. Mechanical equipment should be contained within the roof. If this is not possible, a penthouse should be designed to screen the equipment if it is visible from anywhere on Main Street. The penthouse design should reinforce architectural style.



3.3.8.5 Lighting

1. The architecture of the building should be considered when selecting lighting options.
 - a. Style and scale of lighting fixtures should be complimentary to the building.

A tasteful modern building for small offices would work as well.



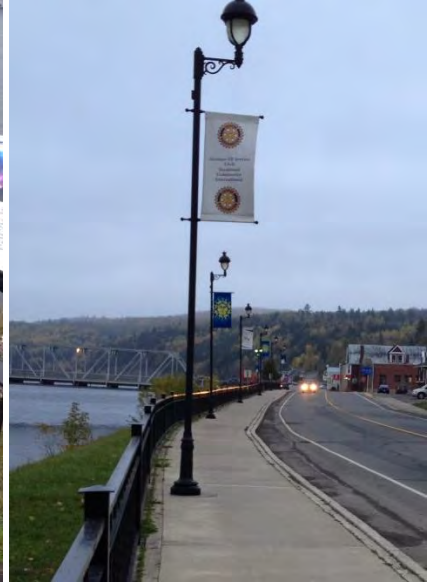
Examples of Traditional Downtown Building Facades and Materials

3.3.9 Street Furnishings

The existing street furnishings create an idyllic rural riverfront downtown. The new vision retains these features. Street trees and light posts with banners would line Main Street. Street furniture has been selected in keeping with the existing aesthetic.



Perth-Andover Downtown Plan



Existing street furnishings

3.3.9.1 Proposed Site Furnishings



Bench by Maglin, Model #MLB100



Trash Receptacle by Maglin, Model #MLWR200



Bike Rack by Maglin, Model #MBR200



Single Water Fountain by Trystan





Tree Grate by Trystan, Model #TU28



3.4 Illustrations of the Conceptual Design



The newly relocated Perth Main Street would be set back 100 feet from its current location.



As one arrives in the downtown from the bridge, one is greeted by the open and inviting Library Plaza.





Aerial view of Library Plaza with an attractive balance of hard and soft surfaces, open and shaded gathering spaces.



Typical streetscape view with connection to riverfront green and St. John River



Perth-Andover Downtown Plan



Example of outdoor dining patio along Main Street



Transition from Commercial Core to residential lands with small townhouse development on the south end





View of Station Rd. Parking lots provide ample space for people coming downtown with plenty of pedestrian connections through to Main Street. A landscaped sidewalk makes Station Road a pedestrian friendly street.

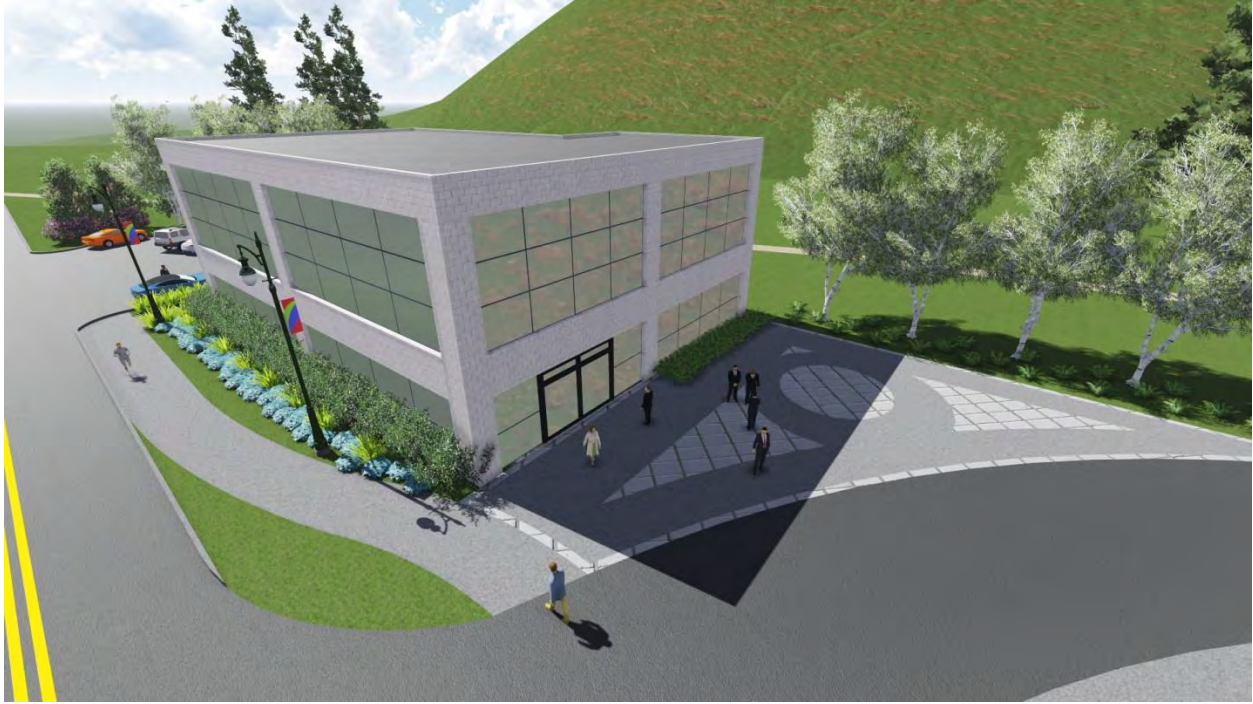


An urban plaza on the north end of the village provides space for picnic, social gathering and passive recreation through a balance of hard and soft surface.





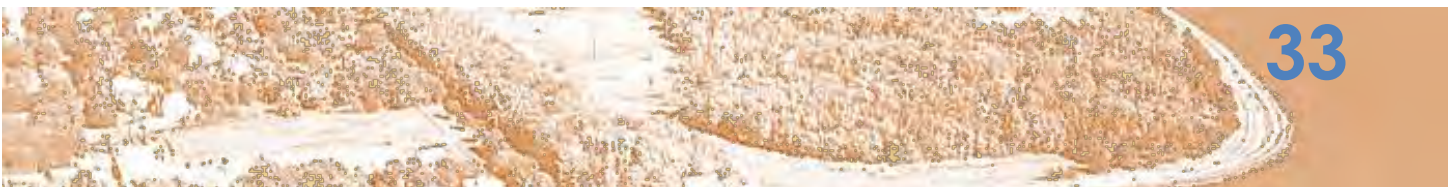
View of urban park. This space is connected to both the riverfront and NB trails with pedestrian crossings to both. Parallel parking lines both sides of Main Street giving easy access to all amenities.



Proposed professional office building should be maximum 3 stories and can accommodate multiple practices.



A cross section showing new downtown from Station Road to Riverfront green including urban parks, retail core, landscaped sidewalk and new Main Street with parallel parking.



4.0 Logistics- Staged Construction

This report describes a vision for both private and public investment in Downtown Perth-Andover. The public components will be implemented in partnership with the province and BIA. Private sector projects will require more time and detailed planning to assemble land and develop proposals. This implementation strategy will focus on costs, as can be determined with current known information, of the recommended projects for public investment.

In order to redevelop the entire downtown with minimal disruption to commerce, a recommendation outlines a phased construction plan.

1. Demolition of vacant/ irreparable buildings
2. Elevation of Station Rd and construction of parking lots
3. Relocation of existing buildings
4. Construction of new buildings
5. Replace services and elevate/ relocate Main St.
6. Connect bridge to new Main Street
7. Waterfront Green

Detailed design and engineering studies will be required in order to realize the vision. Land assembly is required to achieve the desired densification.



Illustration of Staged Construction



5.0 Opinion of Probable Costs

The tables outline an Opinion of Probable Costs to realize the vision. This plan is conceptual and detailed design will be required to verify costs. The Opinion of Probable Costs includes a 10% contingency to allow some flexibility during detailed design that reflects accuracy of the collected mapping. A budget has also been allotted for design or “soft costs” to cover consultant fees, expenses, surveying and testing during construction. Exact costs will depend upon market conditions at the time of tender and the actual budget applied to the project. The project will need to be constructed in stages to match available budgets. All budgets reflect July 2014 installed prices for all equipment, labour, materials and soft costs. These numbers should be inflated in the following years to reflect market conditions such as the price of oil. It is important to understand that this Opinion of Probable Costs was derived from Conceptual Plans and it will require refinement as the project is designed in more detail. A detailed breakdown is appended.



5.1 Summary

OPINION OF PROBABLE COSTS - CLASS C
1341 - Perth-Andover



Description				
Site Work - Flood Remediation Protocols				
Breakwater Rip-Rap Shoreline				\$ 156,000.00
Fill Material to Raise Elevation				\$ 3,652,500.00
Riverfront Trail				\$ 437,250.00
Total:				\$ 4,245,750.00
Building Relocation & Demolition				
Relocate Buildings				\$ 1,040,000.00
Demolish Buildings				\$ 900,000.00
Total:				\$ 1,940,000.00
New Building Construction				
Misc. Building Development Delevopment				\$ 11,954,500.00
Townhouses				\$ 1,119,000.00
Office Building Construction				\$ 1,652,500.00
Total:				\$ 14,726,000.00
Relocated Main Street				
Demolition				\$71,000.00
Construction of New Asphalt Roadway				\$557,500.00
Street Lighting & Electrical				\$303,000.00
Total:				\$931,500.00
Station & Adjoining Street				
Demolition				\$45,000.00
Construction of New Asphalt Roadway				\$344,750.00
Street Lighting				\$135,000.00
Total:				\$524,750.00
Services				
Services				\$4,000,000.00
Total:				\$4,000,000.00
Plaza's				
North Plaza				\$182,750.00
Central Plaza & Adjoining Spaces				\$302,500.00
Total:				\$485,250.00
Misc. Site Features				
Site Features				\$184,000.00
Total:				\$184,000.00
Andover				
Flood proofing existing business	10.00	ea	\$150,000.00	\$1,500,000.00
Total:				\$1,500,000.00
SUBTOTAL				\$28,537,250.00
25% Contegency (inclusive of design fees)				\$7,134,312.50
Subtotal				\$35,671,562.50
GRAND TOTAL				\$35,671,562.50



6.0 Amendments to Rural Plan

The Rural Plan for the Village of Perth Andover was brought into effect in 1997. While the policies still have application, it is recommended that these should be revisited to make them more relevant to the existing social, economic and development climate of the village. Design standards may also be useful in establishing the overall approach to downtown revitalization.

Rural Plan

Commercial Development policies outlined in the Plan provide for a Business Improvement Area (BIA), better street lighting, and parking and encourage pedestrian movements. Further, the policies speak to encouraging any new developments to locate to the BIA. The findings of studies completed for the Rural Plan in 1997 indicate that there was a general movement away from the downtown “Main Street” to an area adjacent to the highway. While these larger format stores provide a service to the travelling public, the downtown area also serves a function being the “heart of the community” and should receive continued support by the Municipality. The movement from the downtown core to the highway area has been in effect for over 15 years and cannot be attributed solely to the most recent flood.

There are provisions in the existing Rural Plan which would support the development of a vibrant downtown area for the village:

Section 3.2.3 Policies

CDP-1 It is the policy of Council to encourage new commercial development to locate in the BIA. The development of diversified, but compatible uses should be encouraged in this area. The Village should cooperate with the business community to improve the BIA to make it the focal point of the Village and dominant shopping area.

CDP-4 It is proposed that the establishment of appropriate buffers consisting visually impervious planting screen or appropriate fencing be incorporated with commercial development. Commercial buildings and lighting or signs shall be attractive in appearance and be designed to minimize distraction to adjoining residential areas. Fascia signs only will be encouraged; however, high quality free standing signs shall be permitted provided they meet the standards established in the zoning provisions of this Plan.

Zoning

The General Commercial zone is structured to be different to the Highway Commercial Zone and as such appears on the east side of the river, particularly in the area around the bridge. Highway commercial has been permitted to be established in various areas on the west side of the River, away from what would seem to be logically the area adjacent to the highway. This zone permits a variety of uses, most of which would be located in larger format buildings. Given the various locations where this type of zoning is located, it appears that the Village has encouraged these commercial uses over the concentration of development in the downtown core.



Changes which could be made to the zoning portion of the plan include creating a greater distinction between the General Commercial Zone and the Highway Commercial Zone, particularly for permitted uses. For example, uses such as a wholesale store should be restricted to the Highway Commercial Zone. Similarly, automobile sales operations would be better located in the larger lot format zone provided for by the Highway Commercial zone. As well, controlling the minimum and maximum sizes of buildings in both zones would discourage certain uses in the General Commercial zone while encouraging other uses. To accommodate potential densification of development along Main Street, changes to the lot dimensions may be required.

7.0 The Business Case

7.1 Why Have a Downtown?

A downtown is much more than the business centre; it is the social/ spiritual “heart” of the community. A small business community needs to be concentrated for mutual support. People come to the downtown for one destination and subsequently shop at other destinations due to exposure from their proximity. Three quarters of purchases happen this way but a business cannot benefit from exposure unless people have another purpose to be in the area. A pedestrian oriented downtown allows for this opportunity.

Density is an opportunity for businesses to share services, reducing their overhead and allowing for better profitability. As the downtown grows and prospers, it will be an attractive centre for government and institutional services (library, town hall, hospital, etc.) servicing Perth-Andover and the region as a whole.

There is also the benefit of convenience for residents as they can do all their shopping and errands at one destination, thereby reducing the amount of travel required.

Because there is one destination for all shopping and services, residents will all navigate to one place, resulting in social interaction. This strengthens the sense of community as area residents have the opportunity to interact in a common destination. Public open spaces provide opportunities for planned civic events and spontaneous social interaction. The downtown will be supported by ancillary amenities such as the riverfront green and the NB Trail.

A vibrant downtown that is prosperous and fosters social relationships inspires young entrepreneurs to stay and invest in the community. Likewise it will attract new business, tourists and residents looking for a certain quality of life.

7.2 Why Retain the Downtown on the Perth Side?

Historically the downtown has been on the Perth Side of the river. It possesses traditional downtown attributes from its architecture and aesthetic to its riverside location. There is value in preserving the historical centre of the community that has significance for many generations of residents.



Despite an extensive overhaul, there will be cost savings in using and tying into existing infrastructure. It is easier to rebuild an existing downtown than to begin from scratch elsewhere. There is presently a tax base of approximately \$2 million from businesses that are invested in the current downtown. There is incentive for the government to invest in an established tax base.

Physically, the downtown is idyllically located on the banks of the beautiful St. John River, nestled in a valley of stunning forest. The aesthetic of the setting lends to the character of the downtown. When exiting the highway, one approaches Perth-Andover from the opposite side of the river and the downtown is a visible destination. It is direct and visible in its current location. It is also not just figuratively but also physically the centre of the community. This cannot be easily duplicated or replaced.

There is too much meaning and, connection and purpose to the current downtown location to consider relocation. It is the logical location.

7.3 What about Andover?

Recent flooding has severely impacted the businesses that remain on the Andover side of the river. There are currently 10 businesses operating on the Andover side of the river. With the relocation of flooded homes these businesses find themselves more isolated than before.

The scope of this project focused specifically on the development of a long-term vision for the Business Improvement Area- the Downtown. Although it is important for not only commerce but also social and cultural needs to have a downtown, there are a number of businesses in Perth-Andover that fall outside of these physical boundaries. The close-knit business community is unwavering on its commitment to provide a solution for ALL businesses affected by the flooding. There are a number of considerations for businesses on the Andover side.

There are only a few dozen businesses in the village. A small commercial base needs to be concentrated. It therefore is strongly recommended that these businesses relocate to one of the two established commercial centres; the Downtown or the Tribe Rd. Zone. Each business should relocate to the area that is best suited to the type of business conducted. For example an automotive repair would not enhance the downtown but would be well suited for the highway zone. Conversely, a retail store depends on pedestrian traffic and would be well supported in the downtown location where as in the Highway zone it would suffer from the lack of pedestrian traffic and visibility. The village should take measures to encourage relocation to the appropriate zone for the benefit of both the business and the community.

There is support in volume. Relocation of the Andover businesses will benefit both those businesses as well as the existing downtown merchants. As 75% of sales result from walk-in traffic, each new business that is a destination for a consumer attracts potential customers for all the other businesses in its vicinity. Andover businesses will benefit from greater traffic if they relocate to one of the denser areas. With increased density comes economic strength. Eventually this stability will attract new business investment as well.

An extensive investment is being made in flood proofing the downtown. Relocating to the BIA would provide security from future flooding.



Perth-Andover Downtown Plan

Another option is for these individual businesses to stay in their present location and flood proof. Based on the residential program where \$100,000 was awarded to flood proof or relocate, a budget of \$150,000 is recommended for businesses to flood proof or to relocate to the downtown.

7.4 Tribe Road Highway Commercial Zone

Guidelines should be established and a general master plan laid out for the Tribe Road Highway Commercial Zone before further growth continues. Although limited, there is developable land available. Providing direction will ensure that the land is maximized and growth is orderly and land use appropriate.

Although this is already an established commercial zone, it is important both for the community, the downtown and prospective businesses that each business located in the appropriate zone. Retail and service based businesses rely on pedestrian traffic resulting from densification. Such businesses survive in numbers and need to be located in close proximity to one another. These businesses should be encouraged to locate in the BIA. Light industrial and highway commercial type businesses should locate along the Tribe Road. Such businesses include automotive sales and repair, among others. These businesses require more land and do not contribute to the densification of a downtown but would thrive in the Highway Commercial Zone. They are, in their nature, not dependent on pedestrian traffic to generate business and would not benefit from being located in the downtown.

Section 6.0 provides recommendation to help the village direct the appropriate growth of each area distinct from the other.

8.0 Conclusion

For Perth-Andover to prosper it must renew itself as a prime location for business and community services. A walkable downtown is critical to the identity and character of the community. To be sustainable the village needs to invest in higher density development to retain and grow its tax base. A blend of services, retail and entertainment are needed for the downtown to thrive.

In order to invest in the downtown there needs to be a sense of security that it won't flood again. This report illustrates a solution for flood proofing that best represents the community of Perth-Andover. It provides a foundation upon which the downtown can renew itself and thrive once again.

The village must take action now to drive the plan forward and ensure revitalization happens before more businesses relocate.



9.0 Next Steps

1. Council needs to adopt the Perth-Andover Downtown Plan as the official direction for the Business Improvement Area.
2. The village needs to work with the BIA to secure funding for flood proofing and relocation of business in a manner consistent with the Downtown Plan.
3. Once the available funding levels are realized the Downtown Plan must be structured into a rational phasing strategy commiserate with the budget.
4. Detailed design should be undertaken for each phase of the project as the budget is finalized.
5. Detailed design should be led by a Landscape Architect or Planner to ensure the essence of the design is retained in the built design.



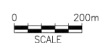
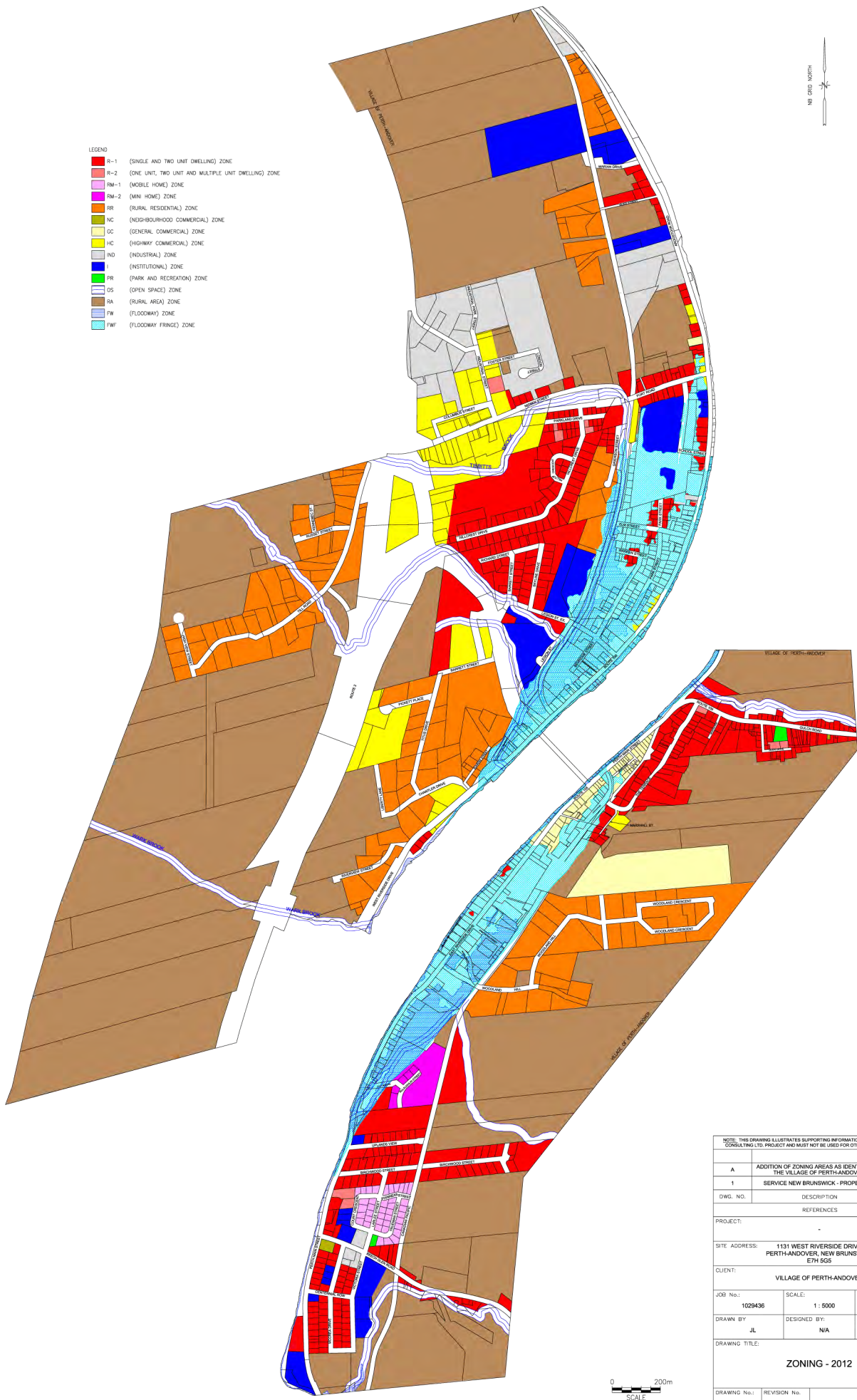
Appendix

Appendix I: Perth-Andover Zoning Map





- LEGEND
- R-1 (SINGLE AND TWO UNIT DWELLING) ZONE
 - R-2 (ONE UNIT, TWO UNIT AND MULTIPLE UNIT DWELLING) ZONE
 - RM-1 (MOBILE HOME) ZONE
 - RM-2 (MINI HOME) ZONE
 - RR (RURAL RESIDENTIAL) ZONE
 - NC (NEIGHBOURHOOD COMMERCIAL) ZONE
 - GC (GENERAL COMMERCIAL) ZONE
 - HC (HIGHWAY COMMERCIAL) ZONE
 - IO (INDUSTRIAL) ZONE
 - I (INSTITUTIONAL) ZONE
 - PR (PARK AND RECREATION) ZONE
 - OS (OPEN SPACE) ZONE
 - RA (RURAL AREA) ZONE
 - FW (FLOODWAY) ZONE
 - FWF (FLOODWAY FRINGE) ZONE



NOTE: THIS DRAWING ILLUSTRATES SUPPORTING INFORMATION SPECIFIC TO A STANTEC CONSULTING LTD. PROJECT AND MUST NOT BE USED FOR OTHER PURPOSES.

A	ADDITION OF ZONING AREAS AS IDENTIFIED BY THE VILLAGE OF PERTH-ANDOVER	2012-06-07
1	SERVICE NEW BRUNSWICK - PROPERTIES	2007-08-15
DWG. NO.	DESCRIPTION	DATE
REFERENCES		
PROJECT:		
SITE ADDRESS: 1131 WEST RIVERSIDE DRIVE PERTH-ANDOVER, NEW BRUNSWICK E7H 5G5		
CLIENT: VILLAGE OF PERTH-ANDOVER		
JOB No.:	SCALE: 1 : 5000	DATE: 2012 06 07
1029436		
DRAWN BY: JL	DESIGNED BY: N/A	APPROVED BY: N/A
DRAWING TITLE: ZONING - 2012		
DRAWING No.:	REVISION No.:	
1	A	



Appendix II: By-Law #54: Business Improvement Area



BY-LAW #54

A BY-LAW OF THE VILLAGE OF PERTH-ANDOVER

TO DESIGNATE A BUSINESS IMPROVEMENT AREA

The Council of the Village of Perth-Andover under authority vested in it by Section 2 of The Business Improvement Areas Act, Chapter B 10.2 R.S.N.B. and amendments thereto, hereby enacts as follows:

1. All that portion of the Village of Perth-Andover more particularly described in Schedule "A" and outlined on Schedule "B" Map, being an area within the Municipality, is hereby designated as a Business Improvement Area.

READ a first time by title this 14 day of November 1989

READ a second time by title this 14 day of November 1989


READ in its entirety in Council this 11 day of December 1989

READ a third time by title and enacted this 11 day of December 1989


MAYOR


CLERK

I certify that the provisions of Section 2 (2) of the Business Improvement Areas Act have been met.


M.E. Watters
VILLAGE CLERK



BEGINNING at a point on the bank of the Saint John River at the North West corner of a lot of land allegedly owned by Village Automotive Ltd. (L.R.I.S. #65043929) thence following in a Southerly direction along said riverbank to a point at the North West corner of a lot of land occupied by the Irving Convenience Store (L.R.I.S. #65044380), Thence Easterly along the Northerly line of this lot to the North East corner, thence Easterly directly across Route 105 to the South West corner of a lot of land allegedly owned by Sobey's Ltd. (L.R.I.S. #65045288), thence Easterly along the South sideline of this lot to the South East corner of said lot, thence in a Northerly direction along the rear or East line of said lot, and Northerly along the rear or East line of a lot of land allegedly owned by Stedman's Ltd. (L.R.I.S. #65044232) and continuing Northerly along the Westerly sideline of a lot of land allegedly owned by Barry Nicholas (L.R.I.S. #65044281) until this line intersects the Southerly right of way of Station Street, thence along said South right of way in an Easterly direction and following the various courses of Station Street Right of Way to the point of intersection of the Easterly right of way of Second Street, thence Northerly along the Easterly limits of Second Street to the North West corner of a lot of land occupied by Carolyn Stewart's Apartment Building (L.R.I.S. #65043861 and formerly known as the Olmstead Building), thence along the Northerly side of this lot in an Easterly direction to the Westerly limits of the CP right of way on the Tobique Line, thence Northerly following the various courses of the Westerly limits of the right of way of Tobique Track, to the intersection of the Easterly limits of Route 105, thence Northerly along said Easterly road limits to the Southerly sideline of a lot of land occupied by Daryl Sadler, thence Easterly along said Southerly line to the South East corner of said lot thence along the rear line in a Northerly direction and following courses of this lot to the North West corner at the Easterly limits of Route 105, thence in a Westerly direction directly across Route 105 and the Tobique Track to the Northerly sideline of a lot of land allegedly owned by Village Automotive Ltd. (L.R.I.S. #65043929) and continuing Westerly along said Northerly sideline to the place of beginning.

The above described area is and intends to be all the area as outlined on the attached Map marked Schedule "B".

Schedule "A"
M. J. White

By-Law #59

**A BY-LAW OF THE MUNICIPALITY OF PERTH-ANDOVER TO IMPOSE
A LEVY ON NON RESIDENTIAL PROPERTY WITHIN THE PERTH-ANDOVER
BUSINESS IMPROVEMENT AREA.**

The Council of the Municipality of Perth-Andover, under authority vested in it by Section 3 of the Business Improvement Areas Act, hereby enacts as follows:

1. This By-Law may be cited as a by-law imposing a levy on non residential property within that area of the Municipality of Perth-Andover referred to as the "Business Improvement Area".
2. A levy of \$0.20 (twenty cents) for each \$100.00 (one hundred dollars) of assessed value is established for the fiscal year beginning January 1, 1998 and ending December 31, 1998.
3. By-Law #67 " A BY-LAW OF THE MUNICIPALITY OF PERTH-ANDOVER TO IMPOSE A LEVY ON NON RESIDENTIAL PROPERTY WITHIN THE PERTH-ANDOVER BUSINESS IMPROVEMENT AREA" enacted December 30, 1996 is hereby repealed.

Read a first time by title this 17th day of November 1997.
Read a second time by title this 17th day of November 1997.
Read in its entirety in Council this 8th day of December 1997.
Read a third time by title this 8th day of December 1997.


.....
MAYOR


.....
CLERK

I certify that the provision of Section 2 (2) and Section 3 of the Business Improvement Areas Act have been met.


.....
M.E. Watters/Clerk

Appendix III: Detailed Breakdown of Opinion of Costs



OPINION OF PROBABLE COSTS- CLASS C

1341 - Perth-Andover

July 9th 2014



Description	Quantity	Unit	Unit Cost	Extended Cost
Site Work - Flood Remediation Protocols				
Breakwater Rip-Rap Shoreline				
Rip-Rap Bank	600.00	Lm	\$160.00	\$96,000.00
Shoreline Planting	1.00	lump	\$15,000.00	\$15,000.00
Misc. Stabilization Materials (Geo & Gabions)	1.00	Lump	\$45,000.00	\$45,000.00
			Total:	\$156,000.00
Riverfront Trail				
Crusher Dust Walkway	1,600.00	m ²	\$35.00	\$56,000.00
Trees	35.00	ea.	\$350.00	\$12,250.00
Seed & topsoil	30,000.00	m ²	\$5.00	\$150,000.00
Lighting	16.00	ea.	\$6,000.00	\$96,000.00
Electrical	1.00	Lump	\$35,000.00	\$35,000.00
Benches & Pad	16.00	ea.	\$3,000.00	\$48,000.00
Trash Receptacles	10.00	ea.	\$4,000.00	\$40,000.00
			Total:	\$437,250.00
Fill Material to Raise Entire City				
Fill Quantity (Rip Rap)	180,000.00	m ³	\$20.00	\$3,600,000.00
Grading	35,000.00	m ²	\$1.50	\$52,500.00
			Total:	\$3,652,500.00
Building Relocation & Demolition				
Relocated Buildings				
Relocation Cost	8.00	ea.	\$ 120,000.00	\$ 960,000.00
Demolition and Cleaning - Previous Lots	8.00	ea.	\$ 10,000.00	\$ 80,000.00
			Total:	\$ 1,040,000.00
Demolished Buildings				
Building Demolition - Small Size	17.00	ea.	\$ 20,000.00	\$ 340,000.00
Building Demolition - Large Size	14.00	ea.	\$ 40,000.00	\$ 560,000.00
			Total:	\$ 900,000.00
New Building Construction				
Misc. Building Development Development				
Building Construction (4500 Sf @ 150 \$/Sf)	17.00	ea.	\$ 675,000.00	\$ 11,475,000.00
Entry & Landscaping	17.00	ea.	\$ 10,000.00	\$ 170,000.00
Parking Lot	5000.00	m ²	\$ 50.00	\$ 250,000.00
Site Furnishings & Misc.	17.00	ea.	\$ 3,500.00	\$ 59,500.00
			Total:	\$ 11,954,500.00
Townhouses				
Building Construction (1200 Sf @ 150 \$/Sf)	6.00	ea.	\$ 180,000.00	\$ 1,080,000.00
Driveway	6.00	ea.	\$ 4,000.00	\$ 24,000.00
Landscaping	1.00	lump	\$ 15,000.00	\$ 15,000.00
			Total:	\$ 1,119,000.00
Office Building Construction				
Building Construction (9000 Sf @ 180 \$/Sf)	1.00	ea.	\$ 1,620,000.00	\$ 1,620,000.00
Building Parking	350.00	m ²	\$ 50.00	\$ 17,500.00
Building Concrete Entry & Landscaping	1.00	lump	\$ 15,000.00	\$ 15,000.00
			Total:	\$ 1,652,500.00

Relocated Main Street				
Demolition of Old Street	6100.00	m ²	\$10.00	\$61,000.00
Demolition of Services	1.00	Lump	\$10,000.00	\$10,000.00
Total:				\$71,000.00
Construction of New Asphalt Roadway				
Asphalt Roadway	8000.00	m ²	\$40.00	\$320,000.00
Concrete Sidewalk	1250.00	m ²	\$65.00	\$81,250.00
Curbing	1500.00	Lm	\$70.00	\$105,000.00
Site Furnishings	1.00	Lump	\$20,000.00	\$20,000.00
Line Painting	1.00	Lump	\$5,000.00	\$5,000.00
Street Planting - Tree's & Pits	35.00	ea.	\$750.00	\$26,250.00
Total:				\$557,500.00
Street Lighting				
Decorative Street Light	30.00	ea.	\$8,000.00	\$240,000.00
Typical Traffic Lights	2.00	ea.	\$6,500.00	\$13,000.00
Conduit & Electrical	1.00	Lump	\$50,000.00	\$50,000.00
Total:				\$303,000.00
Station & Adjoining Streets				
Demolition of Old Street	6000.00	m ²	\$7.50	\$45,000.00
Total:				\$45,000.00
Construction of New Asphalt Roadway				
Asphalt Roadway	4000.00	m ²	\$50.00	\$200,000.00
Concrete Sidewalk	1100.00	m ²	\$35.00	\$38,500.00
Curbing	1000.00	Lm	\$70.00	\$70,000.00
Site Furnishings	1.00	Lump	\$6,000.00	\$6,000.00
Line Painting	1.00	Lump	\$4,000.00	\$4,000.00
Street Planting - Tree's & Pits	35.00	ea.	\$750.00	\$26,250.00
Total:				\$344,750.00
Street Lighting				
Lights	15.00	ea.	\$6,000.00	\$90,000.00
Conduit & Electrical	1.00	Lump	\$45,000.00	\$45,000.00
Total:				\$135,000.00
Services - Abandon, Relocate, and Reinstall				
Lump Sum	1.00	ea.	\$4,000,000.00	\$4,000,000.00
Central Plaza & Extended Plaza				
Hard Surfaces - Central Plaza	800.00	m ²	\$160.00	\$128,000.00
Hard Surfaces - Extended Plaza	600.00	m ²	\$160.00	\$96,000.00
Benches	20.00	ea.	\$3,000.00	\$60,000.00
Fountain	1.00	ea.	\$3,500.00	\$3,500.00
Planting	1.00	Lump	\$15,000.00	\$15,000.00
Total:				\$302,500.00
North End Plaza				
Shack	1.00	Lump	\$60,000.00	\$60,000.00
Surface Paving Stone	400.00	m ²	\$160.00	\$64,000.00
Seating Wall	50.00	Lm	\$500.00	\$25,000.00
Planting	1.00	Lump	\$2,500.00	\$2,500.00
Picnic Benches	4.00	Ea.	\$3,500.00	\$14,000.00
Fences	15.00	Lm	\$150.00	\$2,250.00

Pergola	1.00	Lump	\$15,000.00	\$15,000.00
			Total:	\$182,750.00
Misc. Site Features				
Village Signs	2.00	ea	\$10,000.00	\$20,000.00
Intreprative Signs	6.00	ea	\$6,500.00	\$39,000.00
Road Signs	1.00	Lump	\$15,000.00	\$15,000.00
Art Work	1.00	Lump	\$100,000.00	\$100,000.00
Patio for future expansion	1.00	ea	\$10,000.00	\$10,000.00
			Total:	\$184,000.00
Andover				
Flood proofing existing business	10.00	ea	\$150,000.00	\$1,500,000.00
			Total:	\$1,500,000.00